

CHAPTER FOUR

Roadway Cross Sections

INTRODUCTION

A major consideration in the Auburn Boulevard Specific Plan will be the type of improvements within the public right-of-way. This will affect property access, sidewalks, utility undergrounding, landscaping, bike lanes, travel lanes, and medians. Existing streetscape conditions vary dramatically along the length of the 1.5-mile segment of Auburn Boulevard, as shown in sample existing cross sections below. Due to the varying conditions on the adjacent private property, no single new cross section will be appropriate for the entire Boulevard. This chapter presents a range of possible options for future cross sections, which are presented here to inform a discussion of City priorities and objectives for future streetscape improvements.

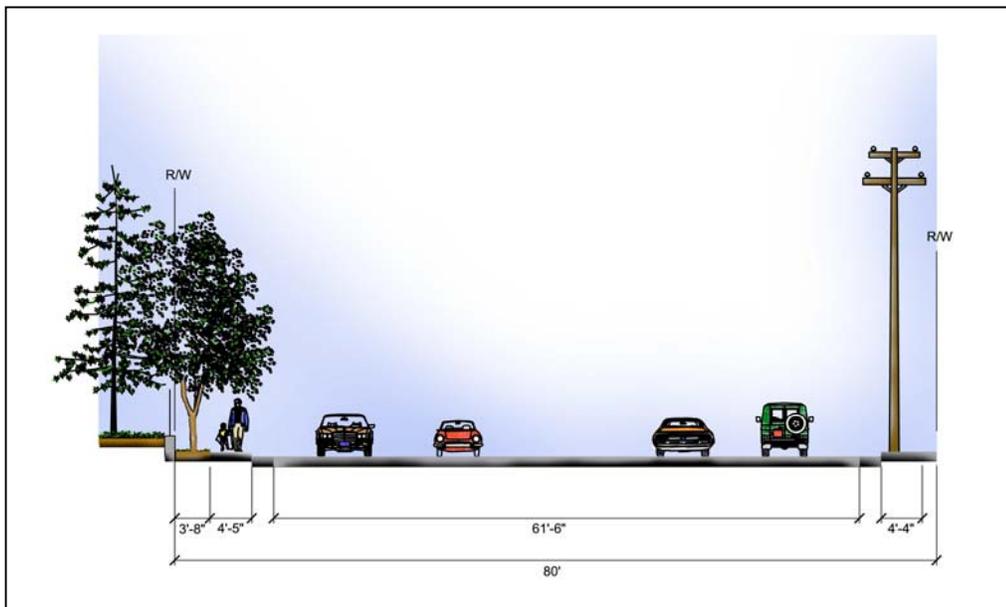
EXISTING CROSS SECTIONS

Sylvan Cemetery (at Maple Avenue)

This section shows a retaining wall and fence along the west side of Auburn Boulevard. Sizeable trees are planted next to the sidewalk and beyond the cemetery wall. A 3-foot, 8-inch strip of dirt between the wall and public sidewalk provides opportunity for more planting. Overhead utilities run along the east side of the Boulevard.

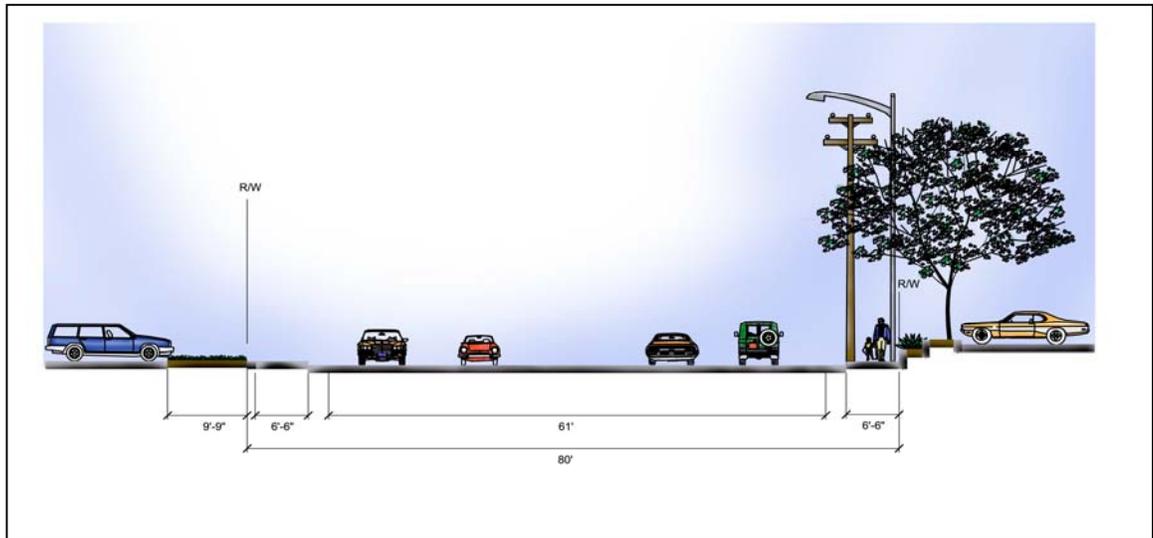


Auburn Boulevard has a variety of streetscape edge conditions.



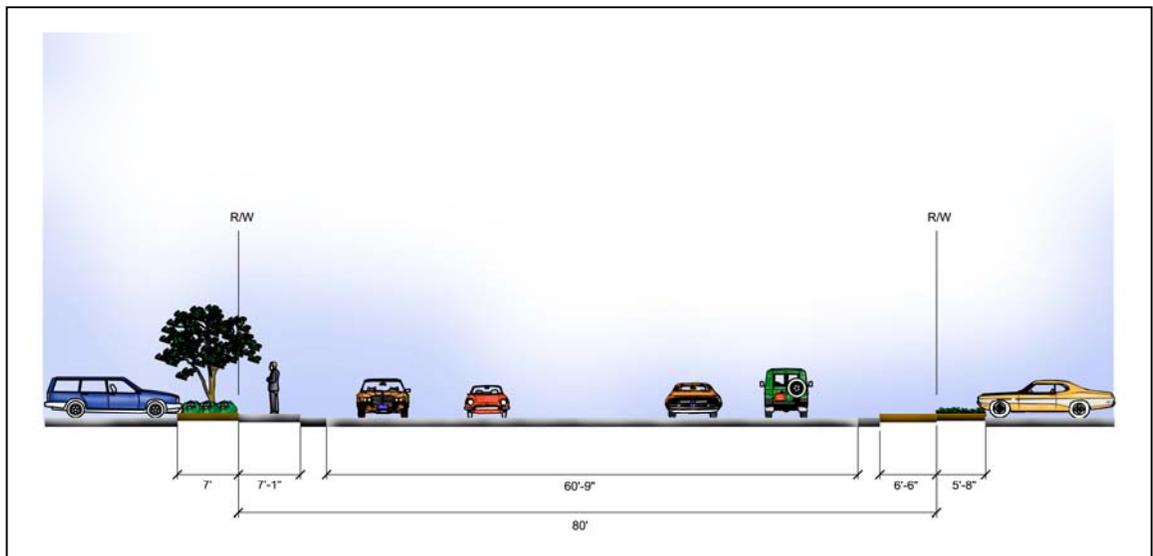
Between Pratt Avenue and Sycamore Drive

This section shows a stepped retaining wall/elevation difference and overhead utilities along the east edge of Auburn Boulevard. Both street edges are flanked by narrow planting strips beyond which are small commercial parking lots.



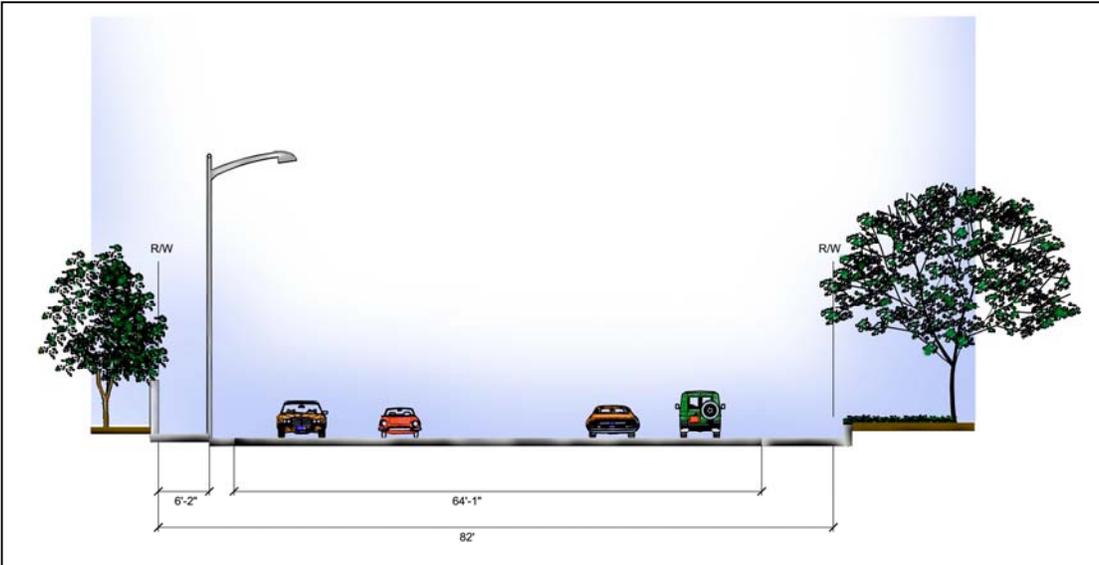
Between Cedar Drive and Baird Way

This section shows the condition at large commercial lots where expansive parking areas could be consolidated. Planting strips on both sides between parking lots and Auburn Boulevard provide an opportunity for adequate street planting.



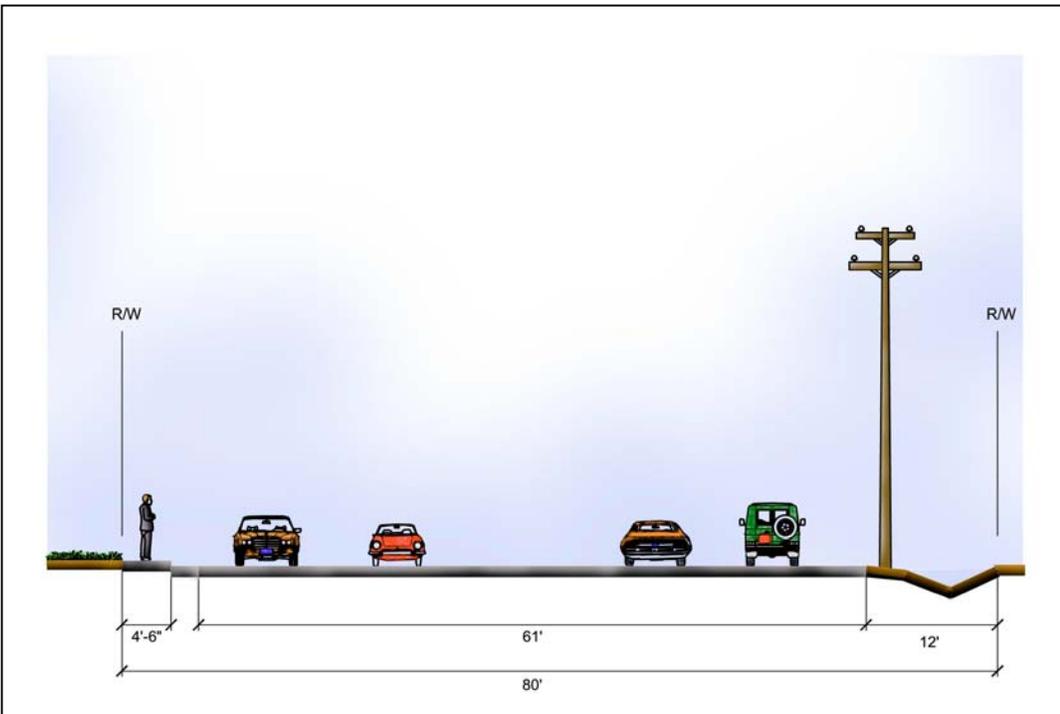
Between Sandalwood Drive and Bellbrook Court

This section shows a sound wall between the road and residential lots along west side of Auburn Boulevard and planted elevated embankment on the east side of Boulevard.



Linden Avenue

This section shows low planting along the west side of Auburn Boulevard and absence of a public sidewalk along the west side.



POTENTIAL FUTURE CROSS SECTIONS

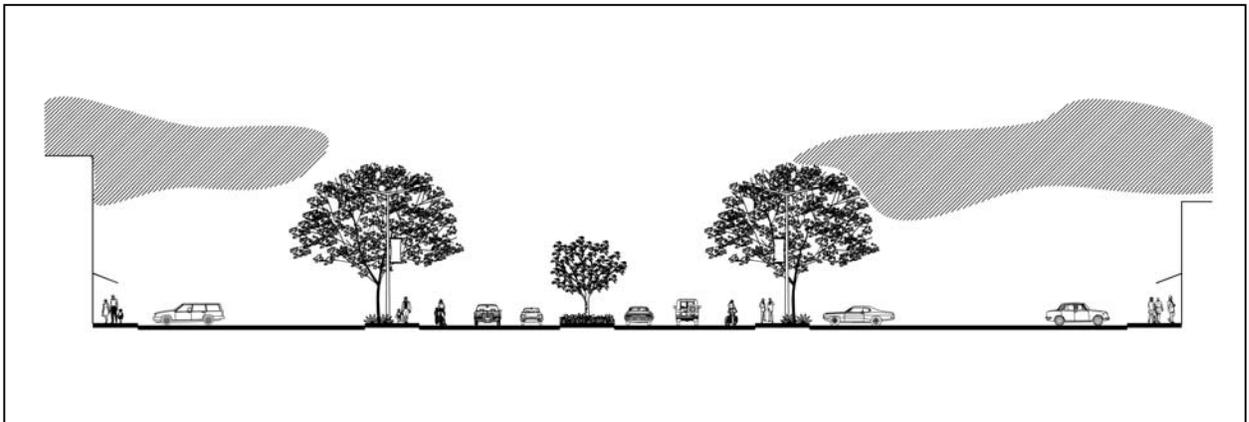
The three cross sections presented below represent a range of possible options and are presented here to facilitate discussion of the City's priorities and objectives for future streetscape improvements. The City Council and Planning Commission will need to give direction on their priorities for improvements relative to the following considerations:

- Width of sidewalk;
- Extent and location of landscaping strips adjacent to sidewalks;
- Inclusion of bike lanes;
- Width of travel lanes;
- Left-turn opportunities for property access;
- On-street parking; and
- Landscaped medians.

No single cross section will be appropriate for the entire length of Auburn Boulevard.

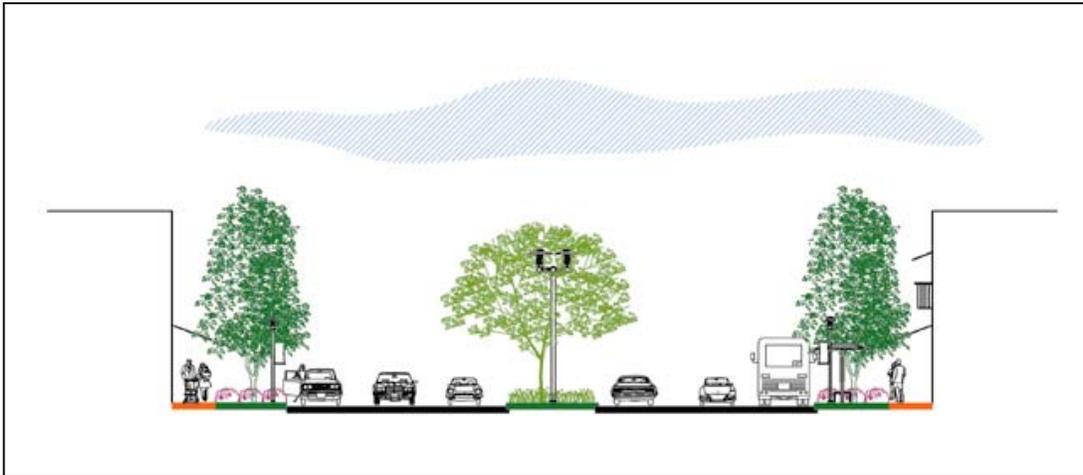
Auto-Oriented Suburban Street

- Buildings and sites oriented towards individual parking lots (not connected)
- “Cobrahead” street and parking lot siting
- Planting on parking lot edge
- Individual parking lots and access drives
- Bike lanes and curb/gutter (used for emergency parking) and 12-foot wide travel lanes



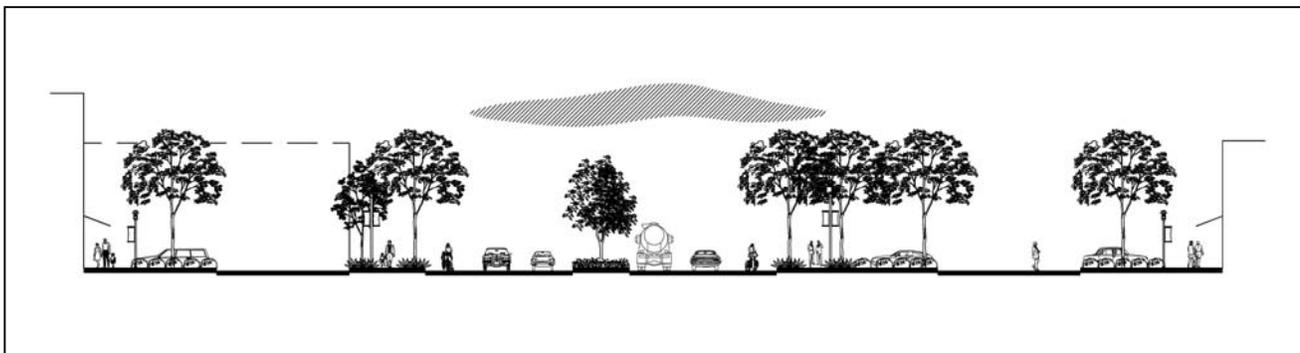
Village Pedestrian-Scaled Street

- Building storefronts oriented toward sidewalk
- Pedestrian-scaled lighting and furniture
- On-street parking and parking behind buildings
- No bike lanes and narrower 11-foot wide travel lanes
- Bus turnouts



Pedestrian-Oriented Suburban Street

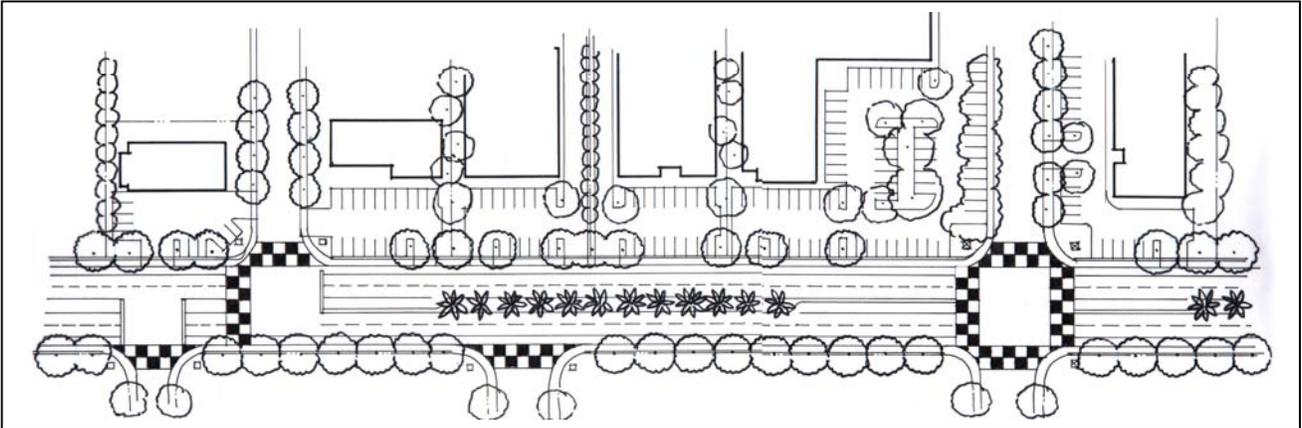
- Interconnected sites and building edges, parking and site landscaping with pedestrian amenities
- Pad buildings along sidewalk edge
- Pedestrian-scaled lighting and furniture
- Detached sidewalk and planting strip
- Parking in front and beside buildings
- Shared access drives
- Bike lanes and curb/gutter (used for emergency parking) and 12-foot wide travel lanes



POTENTIAL STREET IMPROVEMENTS – BIRD’S-EYE VIEW

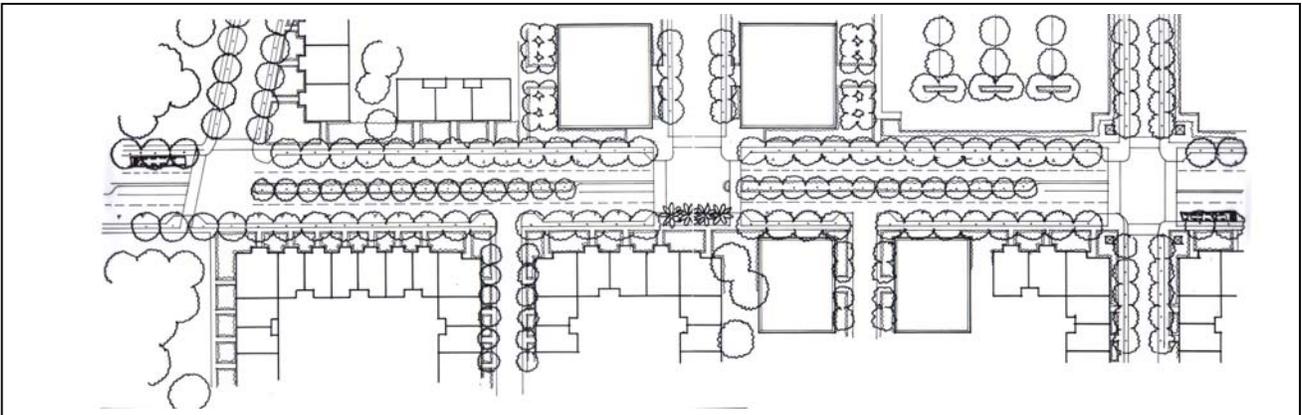
Auto-Oriented Suburban Street

- Continuous street tree planting;
- Accent median planting;
- Thematic signage; and
- Highlighted street crossings.



Village Pedestrian-Scaled Street

- Continuous street tree planting;
- Planted medians; and
- Street furniture.



Pedestrian-Oriented Suburban Street

- Continuous street tree planting;
- Planted medians;
- Street furniture; and
- Highlighted Street Crossings.

